



# Plymouth City Center Design Standards

MAKING CONNECTIONS | MAKING A PLACE | MAKING A NEIGHBORHOOD

# ACKNOWLEDGEMENTS

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Michael Thompson, Public Works Director  
Chris LaBounty, Deputy Public Works Director/City Engineer  
Chris McKenzie, Engineering Services Manager  
Chloe McGuire, Planning and Development Manager  
Lori Sommers, Senior Planner  
Shawn Drill, Senior Planner  
Kip Berglund, Senior Planner  
Alyssa Fram, Recreation Supervisor

## **PROJECT CONSULTANT**

NAC Planning, Steve Grittman and Dan Sjoldal

## **CITY CENTER 2.0 PLANNING TEAM**

Hennepin County, Funding  
GRAEF, Lead Consultant  
Zan Associates, Public Engagement  
BDI, Inc., Market Analysis  
CDG, Bike and Pedestrian Connectivity

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# VISION AND GOALS

## VISION

City Center is a vibrant, safe, walkable area that provides the social hub for Plymouth. It is a mixed-use area that provides places to live, work, play, shop, and eat, and provides the civic heart and anchor for the city.

## GOALS



### Making Connections (The People)

- People can walk and bike safely throughout, and around, City Center.
- People who drive to City Center are encouraged to visit numerous locations after parking once.



### Making a Place (The Feel)

- Create a social hub – an area for food, entertainment and hospitality.
- Create multiple areas for year-round, outdoor activities that can be programmed for active uses that complement existing civic areas and events.



### Making a Neighborhood (The Uses)

- Encourage residential development to support local businesses with a key focus on nighttime and weekends.
- Allow a mix of uses, both vertically and horizontally, with a focus on high-quality architecture.





# HOW TO USE THIS DOCUMENT

## ENFORCEMENT:

- The material in this document illustrates the requirements and standards of the City Center Zoning District and will be applied with the force of those regulations. This document is established as a companion to the Plymouth Zoning Ordinance and the City Center Zoning District (Section 21475).
- This document shall also be incorporated into the 2040 Comprehensive Plan, which is currently referenced as “City Center Design Guidelines” but shall be referred to moving forward as the Design Standards.

These Design Standards are considered an integral aspect of the regulations in the CC, City Center Zoning District. The images in this document are intended to illustrate the intent of the regulations and images and ordinance text may vary from one another. While the images communicate examples of preferred development styles within the District, they are only examples, and creative design elements are encouraged to meet the objectives.

It is necessary that developers and landowners prepare complete projects that incorporate the elements and concepts illustrated by these images, regardless of scale. From the beginning, developers should expect to present complete applications that address each of the required elements in this document (see checklist on page 27).

The city encourages creativity and flexibility in design while still maintaining required elements. The city will provide specific feedback to any proposal when the project requires enhancement to meet the intent of the Design Standards, and/or the specifics of the code. This document is meant to be a “living document” and will be updated over time.

## APPLICABILITY:

The following types of activities require compliance with these Design Standards:

- New building construction.
- Any use change that results in residential units (example: conversion of a multi-story office building, with upper floors converted to residential).
- Modification involving 15 percent or more of an existing building’s exterior street-exposed façade.
- Addition which adds an additional five feet or more of height to an existing building.
- Addition which increases the existing building’s ground floor square footage by 15 percent or more.
- Alteration of the roof massing and roof height of an existing building.
- Site alteration that changes the land cover type of 15 percent or more of the existing site.
- Any application for Site Plan, Subdivision, Conditional Use Permit, Planned Unit Development, Planned Unit Development Amendments. Submittal requirements can be found in the City Code.
- Public infrastructure projects including road rehabilitation projects are referenced in this document, although the Zoning regulations and Design Standards technically apply only to private property. Nonetheless, mirroring the elements of public projects – and designing public projects to fit into the City Center context, is a key aspect of overall District design.

# HOW TO USE THIS DOCUMENT

The Design Standards do not apply to the following activities:

- Replacement, repair, or modification of existing windows or doors with those of a similar size and design.
- Replacement, repair, or modification of roofs with the same or equivalent quality materials.
- Exterior painting.
- Site alterations limited to the repaving or repair of the existing private development site.

As a component of any proposal, applications should include a narrative that explains how the project will be consistent with the code requirements of the CC District, and how the project furthers the goals and policies expressed by this Design Standards document. See the Checklist for Development Projects in this document for more information.

Where any of these standards conflict with city ordinances, the most stringent regulation shall prevail.



## GOALS:

- Enhance bike and pedestrian facilities and connectivity within and connecting to City Center.
- Provide bump outs at all pedestrian crossings of private streets and drive-aisles to minimize pedestrian crossing distance.
- Crosswalks should be designed with a change in material to identify pedestrian crossing areas.
- Encourage a variety of alternative pavements to help delineate different uses (such as patios or sidewalks).
- Pedestrian ways should be given special treatment to highlight their use and distinguish them from vehicle circulation.
- Make extensive use of alternative pavements, colors, and patterns in private parking areas to set pedestrian pathways off from driving and parking surface.
- Private internal walkways should extend over private property to connect building entrances to rear or side parking areas. These walkways should provide pedestrian-scale lighting and 5-foot minimum sidewalk width, and a minimum 6-foot-wide landscape strip at any adjacent building wall.



## REQUIREMENTS:

- Provide connectivity, walkability between buildings and activity centers. All gaps in the pedestrian network must be filled in as development occurs.
- The sloped portion of access drives or driveway aprons shall not intrude into the public realm/streetscape.
- Adjacent parking areas shall have a defined edge from any sidewalk or streetscape area.
- Locate bicycle parking close to the building entrance in a manner that does not impede pedestrian movement.
- Where required, private developments shall pave and dedicate public trail or sidewalk on all adjacent roadways, complying with current city public works standards. Pathways must provide a minimum of 5 feet of width in all locations, including an adequate space to walk or maneuver a wheelchair. trails and sidewalk widths must consider utility boxes, street lights, benches, and other obstructions. public trail and sidewalks shall comply with current public works standards.
- All development projects must provide a pedestrian walking plan, indicating how pedestrians will circulate the site. The pedestrian walking plan will be an integral part of approval, and pedestrians must have a safe and pleasant experience through the site. Pedestrian movements must be prioritized over vehicular traffic, and those parking onsite and walking into the building or off-site must have a prioritized route, clear from obstructions, nuisances (e.g. walking past loading zones or garbage).
- If parking lot connections for shared parking are infeasible, walkways must be provided.
- Walkways may not directly abut a building. A minimum 5 foot landscape strip must be provided.



# MAKING CONNECTIONS: STREETScape

## STANDARDS:

- City Center shall have consistent and expected streetscape improvements within the realm of City Center, including public signage, benches, garbage cans, and lighting. These improvements shall be distinct to the City Center to create a sense of place and continuity throughout the area.
- Developers shall enhance the quasi-public areas in front of their buildings to provide an inviting feel throughout City Center.
- Private projects should put a key focus on integrating their project with the public streetscape.



## REQUIREMENTS:

### Bike Racks

- Placement on the sidewalk areas shall be designed to ensure a minimum clear pedestrian path of at least 6 feet in width.
- Property owners may work with the City to place these in the most suitable location, whether on private or public property.
- Bike racks must be within 25 feet of the building.
- Bicycle parking spaces shall be visible and convenient from the main building entrance.



### Signs

- Developers shall be required to provide the city easement for gateway signage at the entrances/exits to City Center, including parcels on the northern edge of City Center and the southeastern edge of City Center, for the city to install monument signs. If a private development project is at an edge/gateway to City Center and including a monument sign, the project is required to include city signage on that monument sign, when requested.





# MAKING CONNECTIONS: STREETSCAPE

## Exterior Lighting

- All exterior lighting should be designed to maximize the pedestrian use of City Center, focusing on pedestrian pathways, crosswalks, plazas, gardens, building entries, and similar spaces. Nighttime lighting and pedestrian patterns will need to be emphasized for all seasons, to encourage a feeling of safety and ease of wayfinding.
- Parking lot lighting should be limited in pole height, maintaining adequate light levels with more, but smaller, fixtures.
- Decorative lighting should be included and planned for year-round use.
- Lighting shall be pedestrian-scale and meet the Dark Skies Ordinance

## Benches

- Development sites shall provide a minimum of one bench using a City-approved design for each frontage. Locations for benches should be coordinated with entryways, plazas, and similar features, and provide gathering opportunities that cluster benches where appropriate. Pedestrian ways should provide at least 6 feet of clear path, and widened if bench locations require it.
- Benches must be sturdy enough for Minnesota winters, but also comfortable enough for residents.



## Hardscape Elements

- Incorporate limestone edges, walls, and similar features.
- Plan for year-round landscape interest, including holiday décor, summer annuals etc.
- All private streetscape elements, such as small tables, planters, and other seating elements, must be removable and non-permanent. Small tables and planters can be removed to provide opportunities for other outdoor events in this location.
- Utility lines must be buried as part of new development projects whenever possible to reduce the visual impact of utility lines. Utility boxes must be fully screened.
- Developers are required to use city-chosen site furnishings for public areas adjacent to the project and are responsible for ordering and implementing costs.



# MAKING A PLACE: ART

## GOALS:

- The city envisions City Center as a creative district, embracing our local culture through the arts.
- Creating a space for performers and artists to flourish.
- Development projects, both public and private, should support local artists as much as possible to promote local economic growth and foster a sense of community belonging.
- Local art should represent Plymouth's unique community.
- Pedestrian-scale art is encouraged on development projects, including murals, sculpture, areas that encourage people to take photos, and interact with the space.



*This interactive sculpture reads “yo” from one side, and “oy” from the other. People can sit, climb, and take photos with the sculpture that provides a fun sense of place and brings visitors to the area for photos.*

## REQUIREMENTS:

- All new construction projects are expected to commit half of 1% of total development costs to public art, and work with the Zoning Administrator or their designee to choose art consistent with needs.

If it is decided by the Zoning Administrator that art doesn't fit the site, the dollars shall go into a Public Art Fund, which will allow for implementation of coordinated art projects throughout City Center.

- The Public Art Administrator and Zoning Administrator shall engage the Public Arts Commission and local artists on their decisions. As City Center grows, a public art tour or list of interactive locations will be created.
- The Hilde functions as a key arts hub in City Center. Projects in City Center should consider and plan around the Hilde, including items like pedestrian connections to the Hilde and rooftop amenities on projects where outdoor concerts can be heard.
- Blank walls should be transformed in City Center to provide visual interest - which may happen via public art (such as a mural), open space seating, small parklets (see streetscape section), or landscaping (see landscaping section).





# MAKING A PLACE: LANDSCAPING

## GOALS:

- Landscapes are encouraged to provide for a wide range of planting materials, and address multiple seasons of interest, including leaf texture, stem color, flower color, season, and branching habits.
- Trees should be incorporated for shade, or if visibility to building signage is an issue, pergolas or similar shade structures should be a part of the landscape pattern.
- City Center will have lush and abundant plantings that embrace the unique character of the area.
- Private developments are responsible for providing both perennial onsite landscaping and maintaining seasonally appropriate flowers.



*Small street trees and boulevard plantings create a sense of place and slow traffic.*

## REQUIREMENTS:

- Perennial plants should be selected to be fully adapted to the climate and soil conditions, selected to complement other plantings and maximize year-round landscape interest, including species attractive to pollinators, and low-maintenance species to reduce use of pesticides and water. Showy native species are encouraged.
- Plantings must come from the city's approved Tree List or be separately approved by the Park and Recreation Department.
- In this dense area, there likely will not be the area required for significant landscaping. Projects must also implement plantings where able – such as window planters and planter boxes.
- Shrubs and native pollinator-friendly plants are preferred over other plantings and other species that remain in-tact during winter are ideal to create a sense of place in the winter.
- These elements should be an integral aspect of the site and building design package.
- Projects should utilize trellises and vines to cover blank walls.



*Hanging baskets provide landscaping in an urban environment.*

- New private irrigation must be smart irrigation systems.
- The University of Minnesota Extension provides a list of plantings for tough sites that should be utilized throughout projects as much as possible.
- Landscape plans shall depict species growth in ten years. A landscape escrow shall be held for three years for projects in City Center to ensure plant survival.





# MAKING A NEIGHBORHOOD: ARCHITECTURE

## GOALS:

- The objective of the City Center District is for an organic mix of architecture that contributes to an exceptional and vibrant visual built environment.
- The pedestrian-scale of City Center requires active uses on the main floor, as well as significant articulation, visual interest, and high-quality materials.
- Multi-level and mixed-use buildings also add significant benefit to the City Center area, as their mass helps to define and frame the streets. Unique building treatments at street corners and a unified, high quality, design schemes are also encouraged.
- Residential buildings should also address the street, as well as take advantage of possible views across the public open spaces.
- Residential buildings may take different forms, such as townhomes and condominium or apartment buildings, but should always be constructed of high-quality materials and possess varied roof lines and a clear but approachable differentiation between public and private space. They should also include interior parking and provide residents opportunities to access the buildings and relax outdoors, adding to street life and vitality.



## REQUIREMENTS: BUILDING MASSING

- The base or ground floor should appear visually distinct from the upper stories using a change in building materials, window shape or size, an intermediate cornice line, an awning, arcade or portico or similar techniques. The base or ground floor of the building should include elements that relate to the human scale, including texture, projections, door, windows, awnings, canopies, or ornamentation.
- Any building over four stories should include a “step back” of the fourth story and above to maintain the pedestrian scale and prevent a sense of “looming” buildings. Varying building height on such buildings, and varying height in comparison to other nearby buildings, is an aspect of visual interest used to achieve the objectives of the District.
- A building’s roof line can establish its individuality and interest within the context of a commercial area, and variety in roof lines from building to building can add visual interest to a mixed-use area. Some techniques that add interest include varying heights and cornices within an otherwise unified design scheme, using roof line changes to note entrances or commercial bays, and establishing contrasting roof lines at street corners.
- Vary materials and colors. Reflect building materials in the site elements and vice-versa. Consider interior lighting impacts when facing public spaces.
- Make positive use of roof-top spaces or penthouse balconies.
- Buildings shall be visually interesting, including utilizing a mix of materials, highlighting entrances, and utilizing unique pavers.





# MAKING A NEIGHBORHOOD: ARCHITECTURE

- Building design must address both the site, and the surrounding context of surrounding properties and the vision of City Center.
- Buildings shall consider the pedestrian environment in a visible way both in terms of use and distinctive architectural elements.
- Awnings may encroach over the public right of way, and shall maintain a minimum clear height of no less than seven feet over any public or private sidewalk.
- Architecture needs to positively face the street-level visitors and passersby; it needs to differentiate between vertical changes in use when the building is designed for a mixed-use environment, and it needs to create a visually appealing upper-floor.



## MATERIALS AND DESIGN:

- All buildings should be constructed of high-quality materials. The materials must be integrally colored and may consist of brick, natural stone, pre-cast concrete units or glass. Accent materials may include door and window frames, lintels, cornices, architectural metal work, glass block, copper flashing, or similar materials.
- Mechanical equipment must be fully screened in an architectural manner consistent with the building design.
- Entryways should be prominent architectural features, as well as landscape features.
- Building walls that face the street or a public pedestrian walkway or trail shall not have a blank, uninterrupted length exceeding 50 feet for non-residential, mixed-use developments and multi-family developments without including at least two of the following elements: windows, change in plane, change in masonry or pattern, and/or other that breaks up the wall into smaller sections with visual interest.
- All ground floor front building facades shall include display windows to allow pedestrians to view goods and activities inside and encourage walking. Windows shall be clear glass with a visible light transmittance of at least 80%.
- Windows shall cover a minimum of 50% of the ground floor façade facing public streets and public pedestrian walkway or trail.





# MAKING A NEIGHBORHOOD: ARCHITECTURE

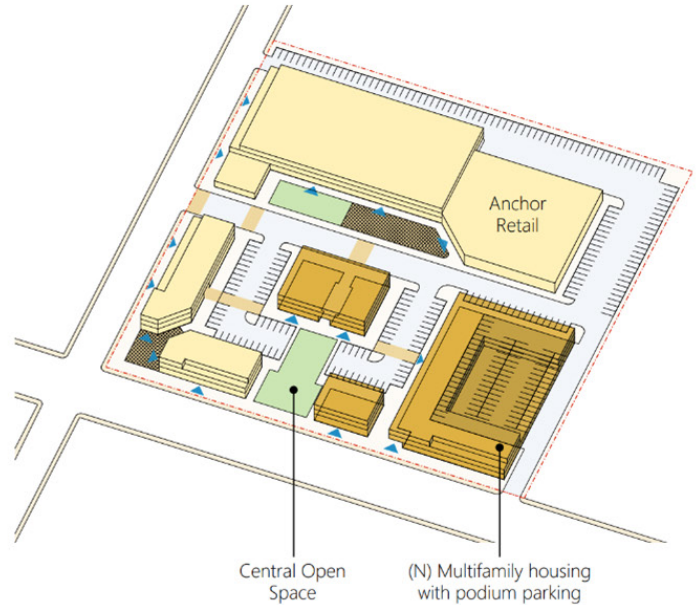
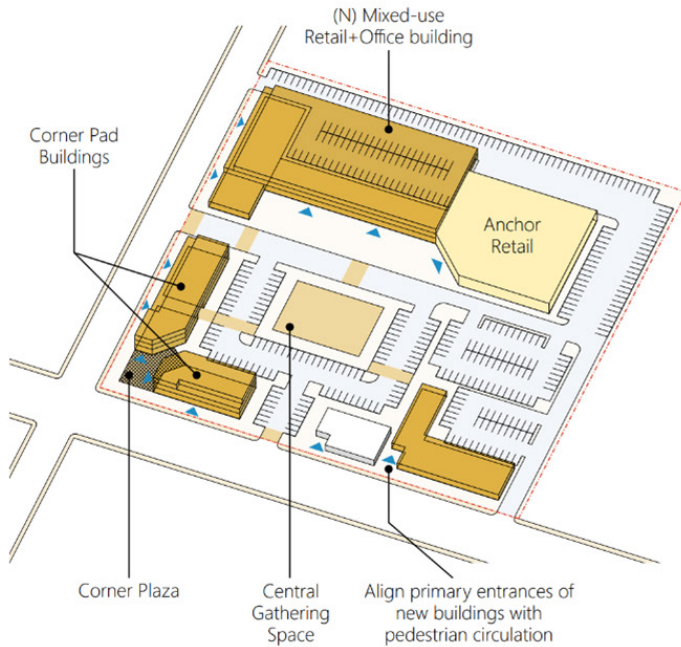
## FACADE:

- Windowsills of any display window shall be no more than 24 inches from the ground.
- Frosted, tinted, black or reflective mirror glass are prohibited.
- Windows on the ground floor shall be clear of opaque films or signage in no fewer than 75% of the individual window frames.
- The minimum unobstructed depth from the required window to the closest parallel interior wall shall be 10 feet.
- Awnings must be of durable materials, and complement the building design, color and materials. Plastic or internally lit awnings are not allowed.





# MAKING A NEIGHBORHOOD: SITE ELEMENTS



## GOALS:

- The way the buildings relate to the street and pedestrian space/streetscape is key to creating a sense of place in City Center.
- To create a memorable environment, structures must have connections into the active interior portion of a building from the sidewalk. This creates a building that actively engages the pedestrian.
- Sites should be seen as an opportunity to fill in gaps in the built environment to create a building wall to provide a more sense of place in City Center.
- Vehicles are fundamental in the use of the district but should not be the first thing that visitors see. The visual dominance of vehicles should be reduced through street design, shared parking, and connectivity. Vehicle parking should prioritize safe pedestrian and bicycle connectivity.
- Create and encourage shared parking.

## REQUIREMENTS:

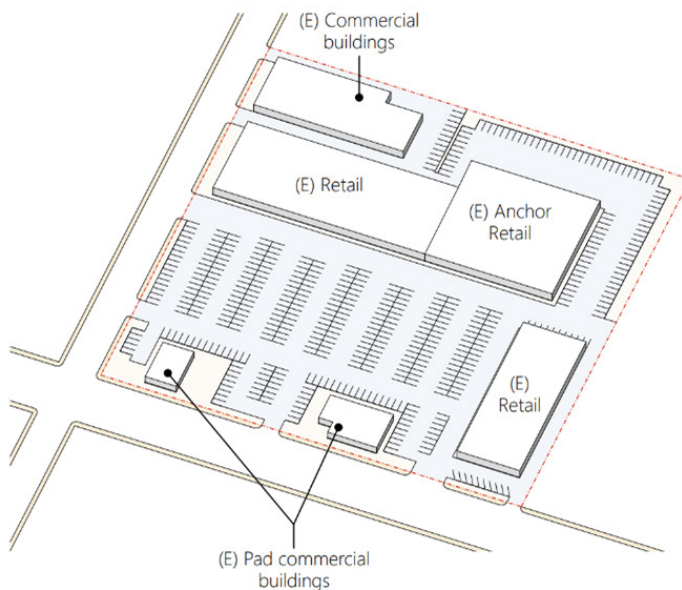
- Site circulation should anticipate future connections to adjacent properties where street or pedestrian connections are not currently feasible such as shared access drives, shared parking, and shared sidewalks.
- Commercial uses should provide a setback between the sidewalk and building sufficient to accommodate outdoor dining and seating areas.
- Buildings adjacent to open space shall provide the primary entrance facing the open space at the sidewalk level. The buildings windows and balconies shall also be oriented towards the open space.
- Consider the design of outdoor spaces around buildings, as both visual, but also as useful spaces that employees, customers, and other visitors to enjoy.



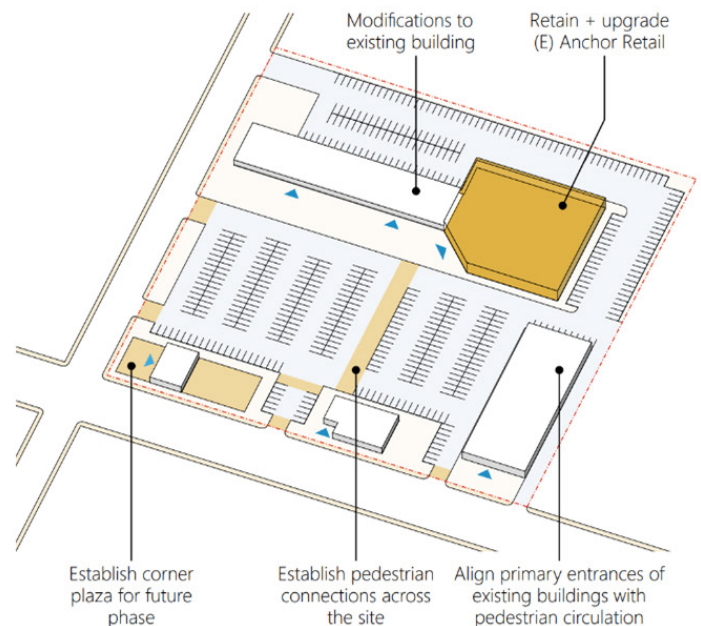


# MAKING A NEIGHBORHOOD: SITE ELEMENTS

- Each development must establish visual and physical connections to the street.
- Building facades must have scaled elements to promote pedestrian comfort, safety and orientation.
- No façade shall be blank.
- Refuse/trash areas shall be located within the primary building.
- Buildings should be placed out to the sidewalk at intersections to define the intersections. In some instances, setting the building back from corner to create a unique location for open space, art, or plaza. The design of the building should focus on the street intersection for the benefit of the pedestrian.
- Parking shall be located behind the building, with structured or underground parking preferred.
- Building shall occupy a minimum of 50% of the street frontage.
- Loading areas shall be exempt from the provisions of Section 21135, provided they are located in areas that are the least visible from street views, and/or least disruptive to surrounding vehicular and pedestrian traffic. Loading areas should be placed away from activity centers and pedestrian areas. Where they are exposed to the public view, the visual impact to adjacent properties minimized through building design or landscaping.
- Service or loading area shall be screened as provided in the zoning ordinance. Loading docks, truck parking, HVAC equipment, trash collection and other service functions shall be incorporated into the design of the building or screened with walls of similar design and materials to the principal building. Landscape material shall also be incorporated to create a screen of at least 6' in height.



PHASE 0: EXAMPLE OF EXISTING COMMERCIAL CENTER SITE



PHASE 1: BUILDING RENOVATIONS + SITE MODIFICATIONS

*Upgrading retained buildings and creating pedestrian connections within the site.*





# MAKING A NEIGHBORHOOD: **PARKING**

## **STANDARDS:**

- Consider reductions in the number of parking stalls required.
- Encourage other means of transportation – walking, biking, and transit.
- Standalone structured parking should contain active main floor uses.
- The majority of parking for residential uses shall be provided within structured parking.
- Surface parking should be supplemented with landscaping and sustainability initiatives such as solar-ready rooftops, EV ready spaces and structures, rain gardens as plantings around surface lots, permeable pavers, internal bike parking, priority compact parking stalls.
- A landscaped buffer strip of at least five feet, or a seating wall, shall be provided between surface parking areas and all sidewalks or public streets.
- Extensive breaks in parking fields should be created by planting areas that provide shade, capture stormwater, and limit long views of pavement. Alternative pavements help delineate circulation and parking spaces, and in some cases can help manage stormwater through innovative materials.
- Design for pedestrian ways within parking areas that minimize walking in drive aisles.
- Emphasize pedestrian ways through private and public areas. Pavement markings should be supplemented by alternative colors or materials.
- Construct green spaces in parking lot to separate rows of parking, rather than end-of-row planting islands.
- Parking lot frontage on pedestrian streets must be reduced, and their edges and interiors should be extensively greened with a combination of hedges, ornamental railings, walls, bollards, trees, and other methods to screen parking lots from pedestrian spaces.
- Parking garages must have public entries on each side with vehicle entrances/exits. Public entries must be located on the main floor and be spacious, allowing at least 8 feet of plaza/public access into the building.



# MAKING A NEIGHBORHOOD: PARKING

## REQUIREMENTS:

- Large expanses of surface parking is not allowed.
- Parking structures, that are below grade or incorporated into buildings, should be located behind buildings to the maximum extent feasible.
- Parking structures shall be designed with architectural treatments that emphasize materials, corner building features. Blank concrete facades will not be allowed. Horizontal banks of concrete are not allowed. Facades must be designed with architectural details like the principal building.
- The parking structure facades should express top, middle, and base components to create distinctions between levels, and minimize the visual impression of a monolithic structure.
- Parking structures entrance drives should be located to minimize conflicts with pedestrian traffic.
- Front yard and corner parking locations are to be minimized.
- EV chargers may not include digital displays, off-site signage, advertisements, or sound (unless for ADA purposes).
- Parking shall include, at a minimum, one of the following sustainability initiatives:
  - o EV-ready parking stalls
  - o Solar-ready rooftops on structured parking
  - o Raingarden perimeters on surface lots
  - o Permeable pavers on surface lots



*A mixed-use building with a camouflaged parking entrance leading to rear parking.*



# IMPLEMENTATION EXAMPLES

The following implementation examples show specific areas throughout City Center fully built out in alignment with the vision for City Center.

Each implementation example has a section identifying key elements of the graphic, a section identifying where flexibility would be considered, and a section describing areas where the city would not allow flexibility in the vision.

These drawings are meant to provide a visual that evokes the feeling of City Center, with the spirit and intent of the ordinances and plan.

# IMPLEMENTATION EXAMPLES

## CITY CENTER NORTH

### Key Elements:

- Two significant residential projects that anchor Vicksburg Lane and Rockford Road corners, with interior courtyard surface parking, and significant below-ground and/or ground-level structured parking.
- Shared parking ramp hidden behind key corridors.
- Additional housing options.
- A restaurant in this general location serves the Fitness and Ice Center traffic.



*The southeast corner of Vicksburg Ln & Rockford Rd*



# IMPLEMENTATION EXAMPLES

## Flexibility in Vision:

- The Plymouth Boulevard building could incorporate mixed-use development on the ground floor level.
- A portion of the parking demand in the northerly section of this area would benefit from the proposed parking structure. This structure can provide overflow for the residential units, much of the parking for the commercial sites along Vicksburg Lane (including the restaurant site shown), as well as existing commercial uses in the area.
- 38th Avenue likely will not connect to Vicksburg Lane, but will be analyzed as projects come in.

## Implementation:

- Projects should include extensive attention to making architectural statements and provide intensive landscape and hardscape elements.
- Connections through and to the adjoining pedestrian and bicycle circulation system.
- Restaurant locations serve to anchor the string of commercial uses along the major roadways they face, as well as taking advantage of the adjoining population base in the adjoining residential buildings.
- Stormwater treatment area designed as a site amenity (as well as its more functional role).
  - o Connecting to other such treatment in the district, these areas can serve a combined utility and open space purpose. Development proposals made for the area should expect to incorporate stormwater design that can accommodate this plan, both from a drainage and landscape perspectives

# IMPLEMENTATION EXAMPLES

## PLYMOUTH BOULEVARD AND 36TH AVENUE

### Key Elements:

- Infill of existing underutilized areas for either new buildings or parking structures
- Intensive mixed-use development along the Plymouth Boulevard frontage of the Ice Center
- Infill along 36th Avenue emphasizes this street as an entry into City Center.
- Viewsheds considered north and east of Plymouth Boulevard.
- Ponding as an open space amenity.
- Development along Plymouth Boulevard can utilize existing grades for structured parking below the principal building, provide commercial frontage and exposure along Plymouth Boulevard, and incorporate convenience commercial services to users of the Fitness and Ice Center facilities. The mixed-use building in the lower center of the view provides one of the best opportunities for such a development.



*View looking east down 36th Avenue at the existing Plymouth Ice Center.*

### Flexibility in Vision:

- The Plymouth Ice Center (PIC)/Lifetime Fitness parking lot is city-owned and is a unique site that could support a variety of uses, including a public parking garage, residential development, an additional sheet of ice, outdoor amenities and activity spaces, or vertically mixed-use buildings.

### Implementation:

- The city-owned parking lot at the PIC/Lifetime Fitness, while a flexible site, should include increased density regardless of use. This area should provide a public amenity, whether that be affordable housing, public parking, an additional sheet of ice, or outdoor activity areas that can be programmed for the city's specific needs. This site could also include a shared parking ramp to serve the area. The city should work with an experienced developer to come up with creative ideas for this parcel.
- Residential development should be incorporated into this area.
- Infill development on smaller and underutilized parcels.

# IMPLEMENTATION EXAMPLES

## NORTH NEIGHBORHOOD – FITNESS/ICE CENTER/PLYMOUTH BOULEVARD

### Key Elements:

- Multi-level parking structure that adjoins the new mixed-use buildings along Plymouth Boulevard.

### Flexibility in Vision:

- Highly flexible area under City-ownership.
- Site could include structured parking, residential, or mixed-use development.
- Site could also accommodate transit stop.
- Park and ride options may be integrated into parking structure.
- Parking structure should be provided in this general area, flexible on location.



*Looking southwest towards Plymouth Boulevard and 37th Avenue from Plymouth Town Square.*

### Implementation:

- High quality, 4-sided architecture regardless of use.
- Structured parking on the PIC/Lifetime Fitness Site.
- Potential for 4th sheet of ice on the PIC building.

# IMPLEMENTATION EXAMPLES

## 34TH AVENUE TERMINUS

### Key Elements:

- The connection of 34th Avenue to Plymouth Boulevard provides one of the major crossings of Plymouth Boulevard between the east and west sides of City Center.
- A significant terminal view for those coming east on 34th Avenue a notable opening in the line of buildings that are shown to front Plymouth Boulevard, and a design which takes its shape from the Hilde Center access location just to the south.



Site looking east at the Plymouth Ice Center/Lifetime Fitness parking lot.

- This layout creates a pedestrian landing area for foot traffic crossing Plymouth Boulevard and focuses traffic to the fitness facility to increase pedestrian and bicycle traffic.
- Infill buildings with frontage on Plymouth Boulevard, with joint structured parking behind and below.

### Flexibility in Vision:

- Highly flexible area under City-ownership.
- Site could include structured parking, residential, or mixed-use development.
- Site could also accommodate transit stop.

### Implementation:

- High quality, 4-sided architecture regardless of use.
- Structured parking on the PIC/Lifetime Fitness Site.
- Density within parking lot site.
- High quality landscaping and treated pavement.
- City partnership with developer who can design site to incorporate numerous uses to meet city goals.



# IMPLEMENTATION EXAMPLES

## ACTIVITY NODE

### Key Elements:

- Open space creating an amenity for commercial uses.
- Restaurant spaces across 34th Avenue to the north.
- Bulk of the uses around the square, at ground level, are encouraged to be commercial in nature. The public or quasi-public uses in this area should complement, and drive traffic to, the surrounding commercial businesses and the Hilde just to the east of this district.
- Pavement surfaces are encouraged to avoid asphalt or typical smooth grey concrete.
- Alternative surfaces, colors, and materials are encouraged in both public and private areas.
- On-street parking as alternative pavement, including permeable pavements to manage stormwater should be evaluated where feasible.



*View to southeast.*

### Flexibility in Vision:

- A key component of implementing this vision as shown above is either the city purchase of a piece of land, or a private redevelopment project that is required to dedicate land to the city for public open space. With a private redevelopment project, the amount of land dedicated to public open space could decrease significantly from what is seen above.
- Commercial spaces should consider rooftop amenities in this area, which provide a unique view of the public open space and the Hilde.
- This area provides a unique opportunity to close the road from the existing movie theater to the Hilde and could provide a location for festivals and events.

### Implementation:

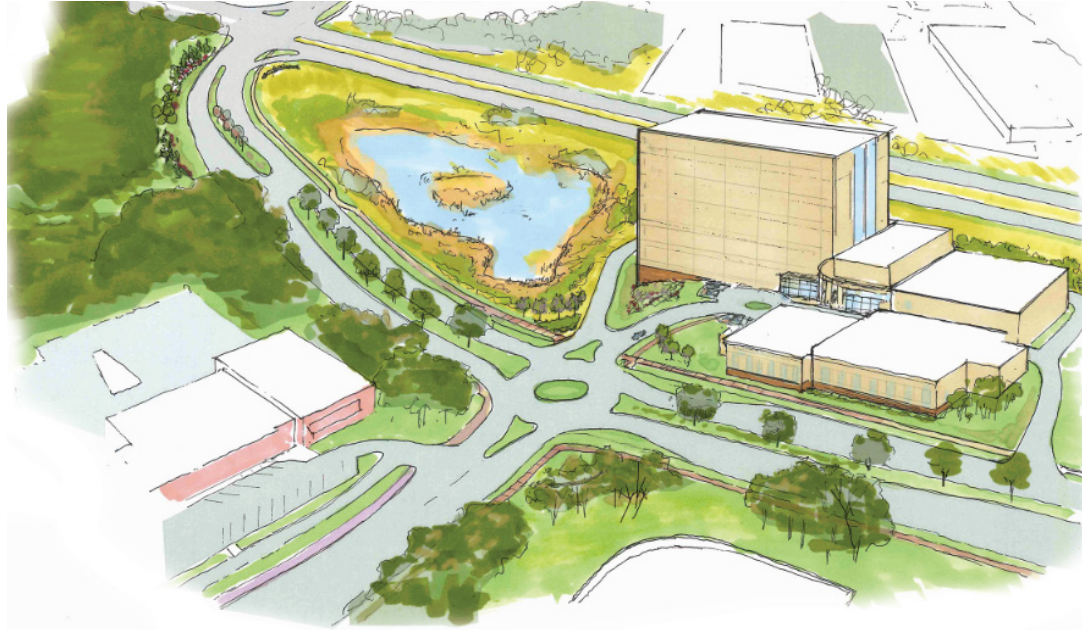
- Implementation of proposed open space involves public purchase of this site, or land dedication through adjacent development.
- Public open space required in this area to provide another gathering space and refuge for pedestrians walking throughout City Center, especially those going to/from the Hilde.
- Require active commercial spaces on main floor spaces on 34th Avenue.
- Uses around this area must consider and provide for connections to the Hilde, as well as adjacent commercial spaces.

# IMPLEMENTATION EXAMPLES

## EAST GATEWAY

### Key Elements:

- Plymouth Boulevard sweeps into the area through a green-flanked landscape to a roundabout.
- The separation of this area – with its valuable Highway 55 frontage – provides an attractive site for a Hotel and Business Center or Convention facility.
- Enhancing the entry drive along 34th Avenue provides an opportunity to create a unique gateway to the City Center area, and the hotel complex helps attract visual attention to the district.
- The area between City Hall and Highway 55 sits lower than the highway, and can accommodate a multi-story facility, with tiers of lower-level structured parking below, without creating an overly dominant building height.



*Looking towards Highway 55 from City Hall.*

### Flexibility in Vision:

- Flexible on uses, overall height, and massing.

### Implementation:

- Increased density onsite, regardless of use.
- Gateway from Highway 55 into City Center. Signage should be considered at this intersection.
- Allow increased height here since no adjacent neighbors impacted.
- Pedestrian crossings to Post Office and City Hall, with an increased trail network.
- Increased high-quality landscaping in this area that provides a lush entrance to City Center from Highway 55.
- Project should be a cornerstone of City Center and will act as a gateway. Project should partner with the city on any additional signage that is needed onsite.

# CHECKLIST FOR DEVELOPMENT PROJECTS

As a part of any individual application requirements provided for in the Zoning Ordinance and those elements listed in the City Center application checklist, projects within City Center shall provide:

- Any new building requires a sketch plan application prior to a full zoning application and review
- Narrative describing how the project aligns with the City Center vision, goals, and requirements
- Planning data including:
  - o Project density calculations
  - o Open space calculations
  - o Floor area
  - o Parking count
  - o Lighting plan
  - o Bike parking count
  - o Pedestrian circulation plan
- Four-sided, eye-level building elevations in color including:
  - o Full screening of mechanical equipment including HVAC, meters, private utility boxes and pedestals, utility lines, and any other such component. Public street signs and light poles must also be shown.
  - o Section diagram showing concealment of any proposed rooftop equipment from eye-level view at adjacent public rights of way.
  - o Building materials and color palette describing proposed materials. Documentation shall include (but not be limited to) color chips for walls, accent materials, trims, awnings, architectural metalwork and samples of visible roofing.
  - o Elevations must have adjacent buildings labeled.
  - o Accurately colored and detailed three-dimensional perspective images (renderings) from at least two vantage points. The perspective images may be waived for small projects at the discretion of community and economic development staff.
- Firetruck turning radius diagram showing how a firetruck can maneuver through the site
- Pedestrian walking diagram showing access from parking structures to building entrances, and building entrances to adjacent sidewalks/trails. Pedestrians must not walk past garbage areas, loading zones, etc.

# CHECKLIST FOR DEVELOPMENT PROJECTS

- Site plans must include:
  - o Standard site plan requirements, including lighting plan.
  - o Light poles, street signs, existing utility boxes, fire hydrants, etc. to ensure clear paths for pedestrians and bicyclists. A minimum pedestrian walkway with a clear path of 6 feet in width will be required.
  - o Vicinity map inset of City Center District.
  - o Proposed building footprints; building walls and openings (doors and windows).
  - o Lot coverage area, impervious surface area, and exterior paving.
  - o Dimensions and materials for any surface parking or structured parking entrances.
  - o Location of any proposed mechanical equipment (e.g. Meters, A/C condensers, back flow preventers, cooling towers, pump houses, electrical transformers), whether ground-, wall-, or roof-mounted.
  
- Illustrated Landscape Plan of the proposed project that includes the following:
  - o Schematic landscape planting plans for all open space
  - o Proposed hardscape location, dimensions, materials, and area
  - o Seasonal variations in site and landscape elements, including planters, structures, etc.
  - o Proposed artwork
  - o Site furniture, traffic control signage, utility equipment cabinets and similar elements
  
- Internal building plan including:
  - o Residential units by square footage and bedroom count
  - o Residential common amenity areas
  - o Commercial square footage, gross and net
  - o Elevators, stairs, and building circulation
  - o Trash and recycling rooms and exterior access areas
  - o Bike parking
  - o Mechanical and rooftop equipment
  
- Any other forms associated with other applications. This is an extra set of requirements in addition to the standard site plan, preliminary plat, CUP, etc. applications.